



SEA BOATS

MARINE BROKERS WITH A GLOBAL APPROACH

AUSTRALIA - BULGARIA - INDIA - INDONESIA - ITALY - MALAYSIA - NEW ZEALAND - PANAMA - PHILIPPINES - SINGAPORE - SPAIN - UK - USA - VENEZUELA - VIETNAM

sales@seaboats.net - www.seaboats.net

Vintage (Nautor 43 Type) Fiberglass Sailboat for Sale!



Listing ID - 1656

Description Vintage (Nautor 43 Type) Fiberglass Sailboat

Date Launched 1979

Length 12.95m (42ft 6in)

Beam 4.04m (13ft 3in)

Location Louisiana, United States

Broker Geoff Fraser
geoff.fraser@seaboats.net
+64 21 61 222 5

FIBERGLASS SAILBOAT (NAUTOR 43 TYPE) FOR SALE

GENERAL CONSTRUCTION PARTICULARS:

The vessel was built of all molded fiberglass construction by Nautor in Pietarsaari, Finland during 1979.

Registered dimensions are: Length – 42.6', breadth – 13.3', depth – 9.6'.

The vessel has a model bow, transom stern, single level recessed deck house, and a double aluminum mast. The hull is fitted with a Keel fin and a displacement type hull. Estimated displacement by the manufacturer is 33,000 pounds with estimated 9,000 pounds of ballast. Deck areas are teak covered. This vessel is a typical recreational fiberglass motor sailing vessel. Handrails fabricated of ¾" diameter vertical stainless steel and wire rope or formed pipe surround the deck. Openings are fitted with safety chains.

Deck fittings are located as follows:

Eight (8) each Lewmar or Harken chrome plated hand operated winches
Ten (10) each Schaefer blocks
Giot deck hatches
Stainless steel Genoa Tracks
Sunbrella helm cover
Stainless steel dingy davit
Folding transom swim ladder
Two spare spars
Roller furler headstay
Stainless steel standing rigging
Stainless steel bow pulpit
Six (6) hawse fairleads with Samson post and six (6) 8" cleats

LIFESAVING EQUIPMENT:

There was little in way of lifesaving equipment aboard the vessel at time of survey. One (1) each sixman inflatable life raft was mounted on the stern. One each inflatable RIB dingy was on board with a 15 horsepower Suzuki four stroke gasoline powered outboard motor.

FIREFIGHTING EQUIPMENT:

Fire fighting equipment consists of four (4) each 2-1/2 lb. dry chemical type fire extinguishers located in the main salon and one (1) 2-1/2 lb. Halon fire extinguisher located in the engine compartment.

HULL:

The hull is constructed of hand laminated one (1) piece fiberglass. The hull is divided by fiberglass and/or wood bulkheads or partial bulkheads into six (6) compartments.

Compartmentation is as follows starting at the bow:

Forward compartment is a chain/rope locker forepeak

Second compartment is a recessed V-berth forward of salon.

Third compartment is recessed head and storage areas

Fourth compartment is the main salon with recessed quarters and head.

Fifth compartment is machinery space and aft cabin.

Aft is the recessed steering area with cockpit drain/lazasrette.

ACCOMMODATIONS:

The single level deckhouse is constructed of one (1) piece molded fiberglass with end grain balsa wood sandwich core. The doors, sky-light, and hatches are fabricated of teak and designed weathertight. The cabin is fitted with ten (10) each bronze hinged opening port lights each with stainless ring and screen. Walls are of spruce construction and teak veneer. Cabin sole is varnished teak and light wood. Overhead is vinyl over spruce wood. Accommodations are partially recessed into the hull and arranged as follows starting at the bow: Located forward is a double V-berth with cushions, storage drawers and wooden lockers below and above.

Next aft to starboard is the teak finished head with marine toilet with holding tank, lavatory, shower, and teak medicine cabinet. Across the small passageway is a storage area with cabinets and shelves. Next aft is the main salon area with port and starboard settees, fold down table and open or enclosed storage areas to port and starboard. Aft to port is the galley with three-burner butane stove with electrical solenoid override, double stainless sink, wooden cabinets with Formica tops, and ice box. Aft to starboard is the navigation area with teak chart/salon table, seat, radar, VHF and HF radios and GPS, and the main electrical panel. Aft of this area and still fully recessed is a cabin with one double and one single built in bunks, integral storage and a fully enclosed head with shower, toilet, lavatory and medicine cabinet. Access to the machinery space is below deck through an insulated hatch. Aft of the main salon is an open cockpit with binnacle steering. The binnacle contains a steering wheel, main engine clutch and throttle controls and a liquid filled magnetic compass. Also located within the cockpit area is an instrument panel with wind speed and direction gauges, speedometer and engine tach. The cockpit contains wood covered seating and hatches for below deck storage. Steering system is mechanical using cables and pulleys. An emergency manual tiller is on board.

RIGGING:

The main mast forward and the second mast aft are each fitted into a socket and constructed of aluminum. Boom is fabricated of aluminum. All standing rigging is stainless steel swaged cable with stainless steel turnbuckles. Running rigging is braided nylon. All sails are Dacron. Chain plates are stainless steel and structurally attached to main hull beams and ribs. The main sheet track is fitted with roller bearing travelers. Genoa tracks are stainless steel with aluminum cars. Rigging is standard for typical 43' ketch rig. Sail area is reported to be approximately 504 square feet. (Sails were all furled.) The hull is reportedly fitted with a one (1) piece internal cast iron ballast bedded into hull reinforced section and glassed over. All rigging attached to deck is bedded and has stainless steel backer plates with stainless bolts and nuts.

ELECTRICAL:

Vessel is wired with marine grade neoprene industrial wiring throughout. Fixtures are of the yacht type. The lighting system appears both 120 volt AC and 12 volt DC. Overload protection is by means of fuses and circuit breakers. Three (3) each 12-volt heavy duty automotive type batteries are on board and stored in plastic boxes in way of the machinery space. Switchboard is of the dead front type designed for recreational vessels. Provision is made for shore power. The vessel does not have a generator.

MACHINERY:

The vessel is single screw auxiliary power. Propulsion machinery consists of one (1) each Model 4.236 four-cylinder Perkins diesel engines driving a single propeller through a hydraulic clutch and reverse gear and a 1-1/4" diameter stainless shaft. Propulsion machinery is electric start and raw water cooled through a heat exchanger. The engine is rated at approximately 85 horsepower at 3200 rpm's. Auxiliary machinery consists of the following:

12-volt shower sump pump

12-volt bilge pumps - two (2) each, one (1) automatic and one (1) manual

120-volt air conditioner

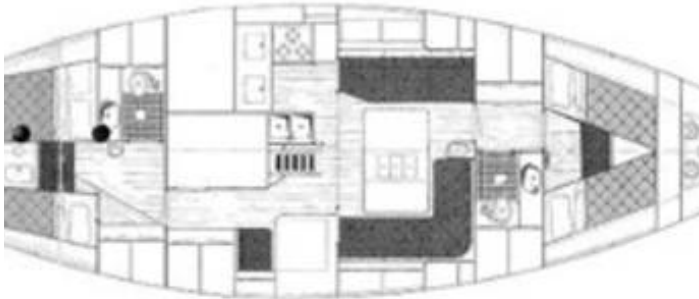
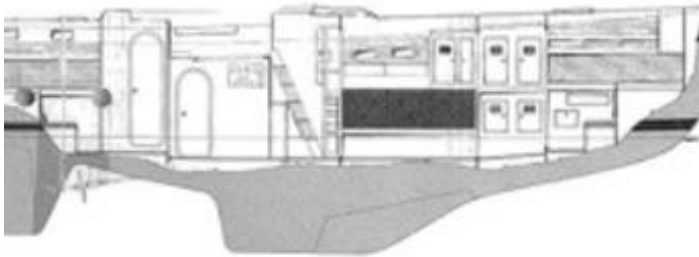
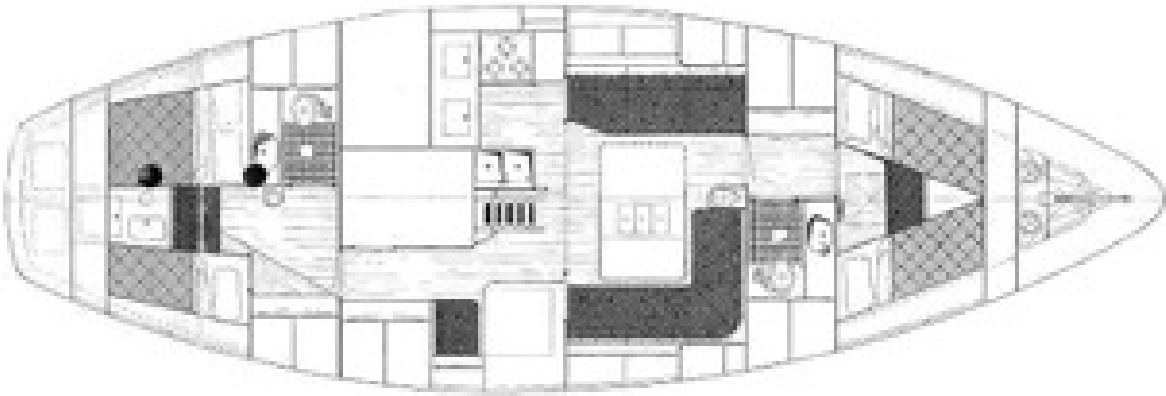
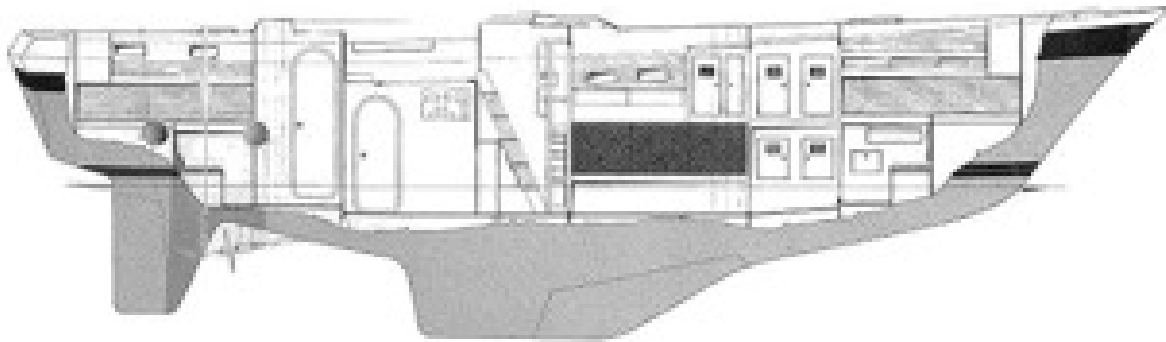
12-volt macerator pump for sewage tank.

TANKAGE:

Below deck tankage located in the bilge of the main salon area consists of port and starboard stainless steel 55-gallon potable water tanks and one (1) each steel 40-gallon baffled diesel tank. A separate fiberglass marine sanitation holding tank with macerator pump is located adjacent to each head. All tanks have approved type fills and vents (with flame screen where applicable). One (1) each 5-gallon butane storage tank is located below deck in way of the aft cockpit. A 12-volt electric solenoid switch is provided.

DOCUMENTATION:

Vessel carries a USCG Certificate of Documentation.





The details of all vessels are offered in good faith but we cannot guarantee or warrant the accuracy of this information nor warrant the condition of the vessel. Any buyer should instruct their agents, or their surveyors, to investigate such details as the buyer desires validated. This vessel is offered subject to sale, price change, location or withdrawal without notice.



AUSTRALIA - BULGARIA - INDIA - INDONESIA - ITALY - MALAYSIA - NEW ZEALAND - PANAMA - PHILIPPINES - SINGAPORE - SPAIN - UK - USA - VENEZUELA - VIETNAM