



SEABOATS

MARINE BROKERS WITH A GLOBAL APPROACH

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NEW BUILD - 23.90m Extreme Exploration Passage Maker (XPM78 V.1)



Listing ID - 3347

Description 23.90m Extreme Exploration Passage Maker

Date Launched Delivery time: 18 months.

Length 23.90m (78ft 4in)

Beam 4.15m (13ft 7in)

Draft 1.34m (4ft 4in)

Location Turkey or Greece (delivery place)

Broker Clive Bennett
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Price USD 2 million

Type: eXtreme eXploration Passage Maker

Naval Architect: NAVAL Studio

Interior Styling: NAVAL Studio

Exterior Styling: Dennis Harjaama / Artnautica NZ

LOA: 23.90m

Lwl: 22.90m

Beam: 4.15m

Draft: 1.34m

Genset (Onan): Optional

Displacement: 45,00 Tons

Full Speed Diesel Model: 14,00 Knots

Cruising Speed : 12,00 Knots

Electric Engines: 1x120 HP Gardner Diesel

Hull & Superstructure Material: Marine Grade Aluminum

FEATURES

LONG

Length; maximum LWL for hull efficiency & speed

Days, high mileage per 24hr day, averaging 240-260 nm days in typical passage conditions

Range, minimum 6000nm @ 10kts range in typical open ocean conditions

Anchorage times, measured in weeks and months not days.

Forepeak for storage only

Engine room with workshop, all aft

LEAN

Beam with wave piercing style bow for maximum "silent and slippery" wave piercing and minimal wave/wake making.

Green energy wise in all regards, propulsion, electrical, maintenance

Remember the 99% Rule: Designed for just the 2 of us 99% of the time

LOW

Physical appearance being long and low in the water with low house and superstructure above decks.

Air draft for as many options of canals, locks, lower bridges

Maintenance Unpainted exterior, minimum stainless

Costs to maintain and operate

Dependencies ashore, maximize self sufficiency

D/L ratio, low displacement to find the "Goldilocks" just right balance of having enough displacement for maximum sea kindliness and yet minimum mass to drive through the water

Height, no double stacked living spaces

Shoal draft less than 1.5m (5 feet) for exploring the shallows

Fly bridge "lite" on roof of cabin with full upper helm station

LIGHT & LITE:

Visual appearance on the water, long, low, lean and mean.

Displacement by saving weight where possible and spending where wise.

Bright everywhere inside, 360-degree light & views in SuperSalon

Open plan galley & salon

LONGEVITY:

Value

Service intervals

Equipment, engine, propulsion, systems

Max fuel capacity for longest range and time between fills

LOOKS:

Matching her/our purpose and mission.

The UN Yacht: unpainted aluminum, no stainless, no wood, no extras.

Blend in at a local working port or commercial fishing harbor and not fit so well in marinas

Strong Industrial/commercial quasi-military "vibe" partly by a design that is long, low, lean and mean" and partly through the use of very high functionality very low maintenance exterior items such as all unpainted aluminum hull and superstructure, beefy aluminum rub rails and booms and other functional "all business" meant to be used features reminiscent of modern pilot boats, tug boats and commercial fish boats than a "yacht".

Starkly contrasting the "lean & mean" exterior, the interior will have an extremely high craftsman level fit and finish of all cabinetry and interior surfaces

ECONOMICAL

Economic to build: Maximized use of 3D CAD modeling, CAM and CNC for all construction, exterior and interior materials (aluminum, wood, fabrics, etc.

Economic to maintain; Careful selection of materials, equipment and installation to minimize maintenance and failures.

Economic to operate: Maximum efficiency throughout.

Minimize size of interior with maximum comfort for 2/4/6/8; Live 2, sleep 4, eat six, entertain 8

2 cabins, 2 heads

Down Up Down design; Raised center house with 360 degrees of glass and cabins down below deck level on each end

Maximum solar output

EFFICIENT

in all regards; hull, engine, propulsion, electrical as well efficiency in use.

Maximum solar panel re-charge ability, sufficient to eliminate need for genset

Low rpm ultra-efficient and robust diesel main engine,

No generator

Battery based boat, maximum size 24v house bank, all loads 240v AC off battery/inverters

SAFE

On passage first and anchor second.

Integral tanks below WL for maximum fuel and counterbalancing water.

Self-righting; Maximum positive stability to "survive and thrive" a capsize. All our past boats were sailboats which are inherently self-righting, and we were not willing to give this up with this transition to voyaging under power.

Unappealing and menacing to others on the outside

Inviting and alluring on the inside

STRONG

Like the Timex watch commercial "take a licking and keep on ticking" self-righting and able to have an inevitable grounding and keep going, no haul out required

Strategically "over built and over engineered" in the just right places with high redundancy of all critical systems

Thrive not just survive

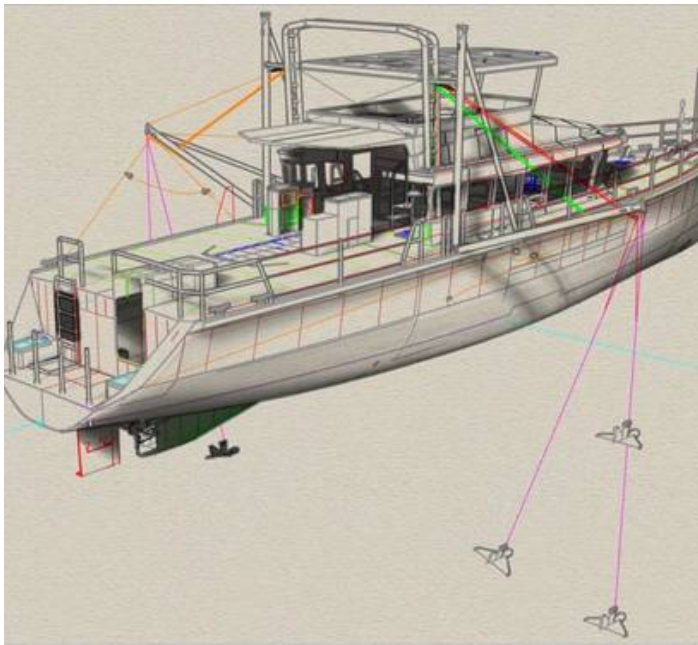
Watertight "crash" bow compartment at front of forepeak

COMFORTABLE

At sea first and foremost, in all weather conditions. Passive stabilization with active designed into hull but not installed at launch.

At anchor, great ventilation in all conditions, flopper stoppers, great seating, lots of light, spacious outdoor areas "Thermos" hull; Cool in hot climates, warm in cold climates.

QUIET: weather, mechanical systems, humans, insulation, vibration.







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